

**Table 6.3 Context per Community Type or Zone**

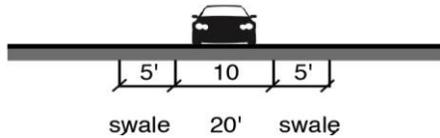
Community Types	Development Intensities	Context
	Natural (N)	Rural
	Rural (R)	Rural
	Low Intensity (L)	Walkable Urban
	Medium Intensity (M)	Walkable Urban
	High Intensity (H)	Walkable Urban
Transect Zones	Natural(T1)	Rural
	Rural (T2/T2L)	Rural
	Neighborhood Edge (T3/T3L)	Walkable Urban
	General Neighborhood (T4/T4L)	Walkable Urban
	Town Center (T5)	Walkable Urban
Use Zones	Rural Density Residential (R5&R5L)	Rural
	Low Density Dwelling (D1&D1L)	Suburban
	Medium Density Dwelling (D2&D2L)	Suburban
	High Density Dwelling (D3)	Suburban
	Mobile Home (DM)	Suburban
	Mixed Use (MU)	Suburban or Walkable Urban
	Neighborhood Commercial (C1)	Suburban
	Community Commercial (C2)	Suburban
	Regional Commercial (C3)	Suburban
	Light Industrial (I1)	Suburban
	Medium Industrial (I2)	Suburban
	Heavy Industrial (I3)	Suburban

**Table 6.4 Thoroughfare Classification Context**

Suburban	Walkable Urban	Rural
Principal arterial	Boulevard	Principal arterial
Minor arterial	Avenue, Boulevard	Minor arterial
Collector	Street, Avenue	Collector
Local	Low Intensity Street, Street	Local

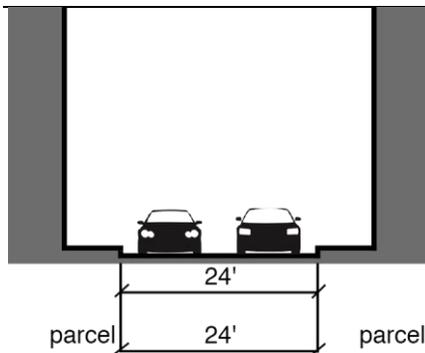
- a. Walkable Urban Thoroughfare Cross Sections per Table 6.5 are required in all community type subdivisions or thoroughfare improvements in transect zones T3/T3L, T4/T4L, and T5. See Table 6.4 Thoroughfare Classification Context.
- b. Suburban thoroughfare cross sections per Table 6.6 Suburban Thoroughfare Cross Sections are required in all use zoned subdivisions with lots less than 10,000 square feet, or any lot less than 80 feet wide at the front building setback, or a gross density greater than 4 dwelling units per acre.
- c. Rural and Natural Zones thoroughfare cross sections per Table 6.7 Rural Use Zone Thoroughfare Cross Sections are required per Table 6.3 Context per Community Type or Zone.
- d. The following documents shall be used to guide the planning, design, and construction of motor vehicle, bicycle, and pedestrian facilities:
  - i. Institute of Transportation Engineers (ITE) Context Sensitive Solutions (CSS);

**Table 6.5 Walkable Urban Thoroughfare Cross Sections**  
**Lane: L-20-10**



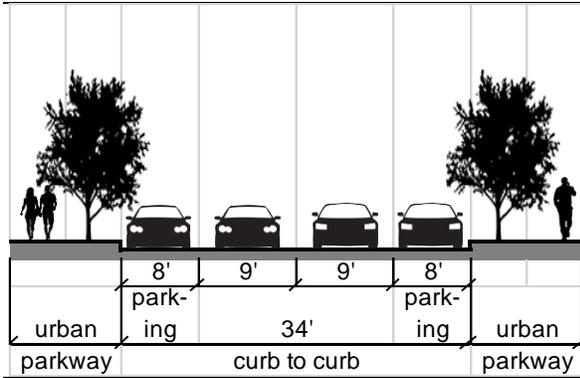
<b>Easement Width</b>	20 ft.
<b>Design ADT</b>	N/A
<b>Design Speed</b>	N/A
<b>Pavement Width</b>	10 feet
<b>Maximum Grade</b>	12%
<b>Minimum Curve Radius</b>	Taper
<b>Clear Sight Distance</b>	20' along local street from end of curb taper
<b>DI &amp; Zoning Districts</b>	L, M, T3/ <u>T3L</u> & T4/ <u>T4L</u>
<b>Functional Classification</b>	Access

**Alley: A-24-24**



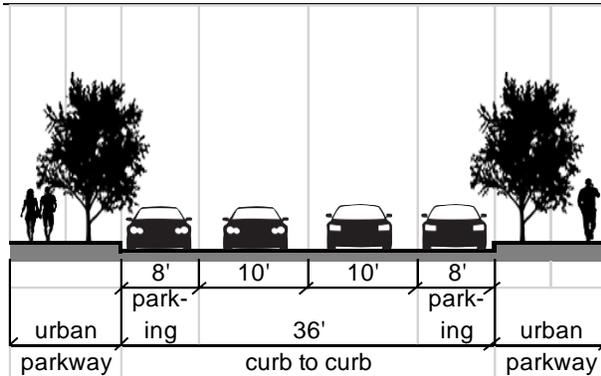
<b>Easement Width</b>	24 ft.
<b>Design ADT</b>	N/A
<b>Design Speed</b>	N/A
<b>Curb-to-Curb Width</b>	24 feet
<b>Maximum Grade</b>	12%
<b>Minimum Curve Radius</b>	Taper
<b>Clear Sight Distance</b>	20' along local street from end of curb radius
<b>DI &amp; Zoning Districts</b>	M, H, T4/ <u>T4L</u> & T5
<b>Functional Classification</b>	Local

**Low Intensity Street: ST-52-34**



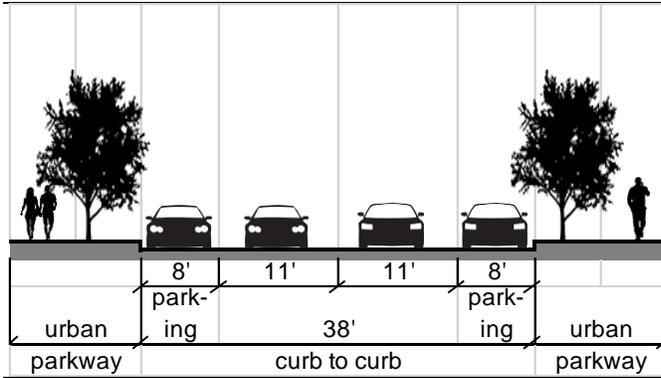
<b>ROW Width</b>	52 ft.
<b>Design ADT</b>	Less than 1,500
<b>Design Speed</b>	15 mph
<b>Curb-to-Curb Width</b>	34 feet
<b>Maximum Grade</b>	12%
<b>Minimum Curve Radius</b>	100 feet
<b>Clear Sight Distance</b>	20' along local street from end of curb radius
<b>DI &amp; Zoning Districts</b>	L, M, T3/ <u>T3L</u> & T4/ <u>T4L</u>
<b>Functional Classification</b>	Local

**Low Intensity Street: ST-58-36**



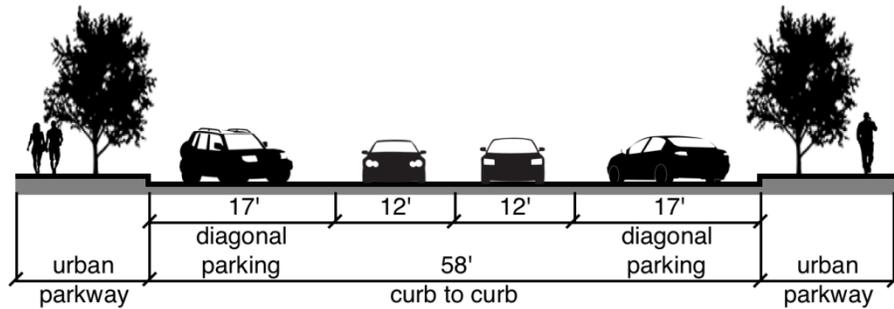
<b>ROW Width</b>	58 ft.
<b>Design ADT</b>	Less than 7,500
<b>Design Speed</b>	20 mph
<b>Curb-to-Curb Width</b>	36 feet
<b>Maximum Grade</b>	12%
<b>Minimum Curve Radius</b>	100 feet
<b>Clear Sight Distance</b>	20' along local street from end of curb radius
<b>DI &amp; Zoning Districts</b>	L, M, H, T3/ <u>T3L</u> , T4/ <u>T4L</u> , T5
<b>Functional Classification</b>	Local, Minor Collector

**Street: ST-60-38**



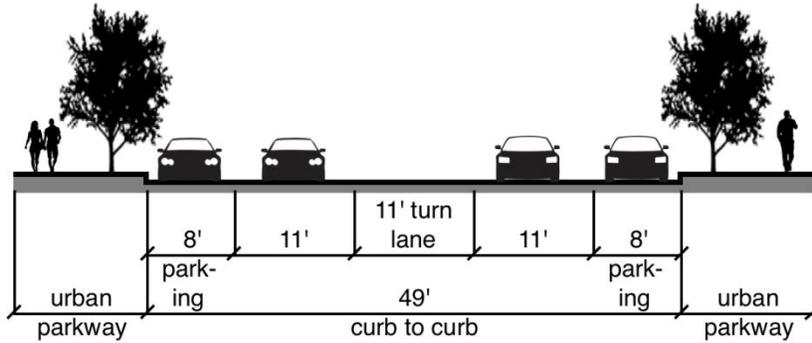
<b>ROW Width</b>	60 ft.
<b>Design ADT</b>	Less than 15,000
<b>Design Speed</b>	25 mph
<b>Curb-to-Curb Width</b>	38 feet
<b>Maximum Grade</b>	12%
<b>Minimum Curve Radius</b>	100 feet
<b>Clear Sight Distance</b>	25' along collector street from end of curb radius
<b>DI &amp; Zoning Districts</b>	M, H, T4/ <u>T4L</u> , T5
<b>Functional Classification</b>	Collector

**Street: ST-82-58**



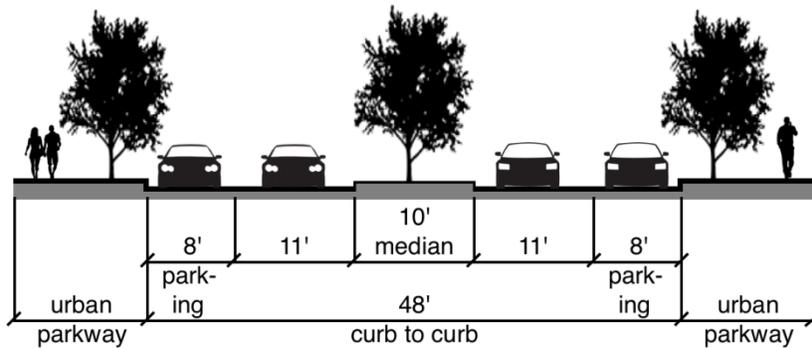
<b>ROW Width</b>	82 ft.
<b>Design ADT</b>	Less than 15,000
<b>Design Speed</b>	25 mph
<b>Curb-to-Curb Width</b>	58 feet
<b>Maximum Grade</b>	8%
<b>Minimum Curve Radius</b>	100 feet
<b>Clear Sight Distance</b>	25' along collector street from end of curb radius
<b>DI &amp; Zoning Districts</b>	M, H, T4/ <u>T4L</u> & T5
<b>Functional Classification</b>	Collector

**Avenue: AV-73-49**



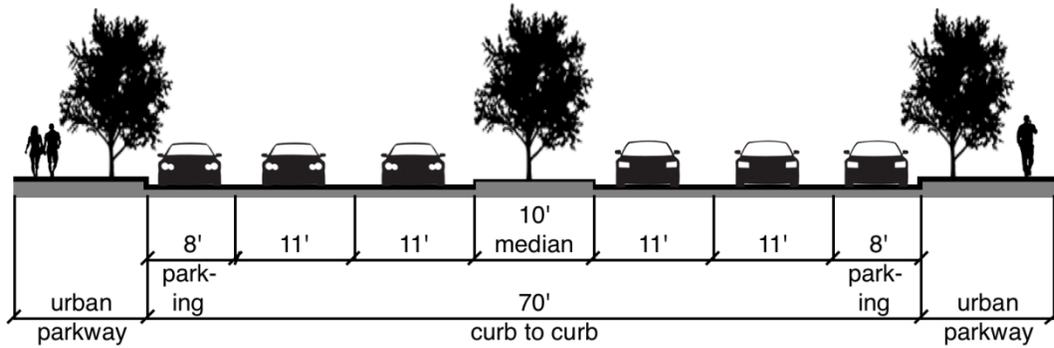
<b>ROW Width</b>	73 ft.
<b>Design ADT</b>	Less than 22,000
<b>Design Speed</b>	30 mph
<b>Curb-to-Curb Width</b>	49 feet
<b>Maximum Grade</b>	8%
<b>Minimum Curve Radius</b>	Varies
<b>Clear Sight Distance</b>	30' along collector street from end of curb radius
<b>DI &amp; Zoning Districts</b>	M, H, T4/ <u>T4L</u> , T5
<b>Functional Classification</b>	Collector, Minor Arterial

**Boulevard: BV-72-48**



<b>ROW Width</b>	72 ft.
<b>Design ADT</b>	Less than 15,000
<b>Design Speed</b>	30 mph
<b>Curb-to-Curb Width</b>	48 feet
<b>Maximum Grade</b>	8%
<b>Minimum Curve Radius</b>	100 feet
<b>Clear Sight Distance</b>	30' along collector street from end of curb radius
<b>DI &amp; Zoning Districts</b>	M, H, T4/ <u>T4L</u> & T5
<b>Functional Classification</b>	Collector, Minor Arterial

**Boulevard: BV-94-60**



<b>ROW Width</b>	94 ft.
<b>Design ADT</b>	Less than 32,000
<b>Design Speed</b>	30 mph
<b>Curb-to-Curb Width</b>	70 feet
<b>Maximum Grade</b>	8%
<b>Minimum Curve Radius</b>	100 feet
<b>Clear Sight Distance</b>	30' along collector street from end of curb radius
<b>DI &amp; Zoning Districts</b>	M, H, T4/T4L, T5
<b>Functional Classification</b>	Collector, Arterial

i. Suburban Thoroughfare Cross Sections

- i. If the right-of-way needed for complete roadway construction is constrained, the cross section should be reduced in the following order:
  - (a) Planting strip width to 3 feet minimum. For Suburban Residential Local, 2 ½ feet are allowed.
  - (b) Sidewalk width to 5 feet minimum.
  - (c) Bicycle lane to 5 feet minimum.
  - (d) Replace bicycle lane with 14 feet minimum wide curb lane.
- ii. Bicycle lanes shall be striped on both sides and include buffer strip and signs, directional arrows, and stencils.
- iii. Bicycle lanes shall not be built on roadways classified below major local.
- iv. All on-street parking decisions shall be determined by the adjoining land uses.
- v. All roadways classified as collector and above may be designated as transit routes. Transit-related facilities, such as a shelter or bus pull-out, may be required.
- vi. Cul-de-sacs require pedestrian and bike access through head, except where a physical barrier exists. Exceptions approved by Zoning Administrator.

**Table 6.11 Parkway Assemblies**

Details	Rural/T2/T2L	L/T3/T3L	M/T4/T4L	H/T5	Suburban
<b>Curb Type</b>	Swale	Raised Curb	Raised Curb	Raised Curb	Raised Curbs
<b>Walkway</b>					
<b>Type</b>	Not required	Path / Sidewalk	Sidewalk	Sidewalk	Path / Sidewalk
<b>Min. Width (feet)</b>	n/a	8 – path 4 – sidewalk	5	6	8 - path 4 - sidewalk
<b>Lighting</b>					
<b>Max. Height (feet)</b>	Not required	12	16	16	20
<b>Min. Spacing (feet)</b>	n/a	At intersections	At intersections	50 on center	At intersections
<b>Planter</b>					
<b>Type</b>	Swale	Bio-Swale	Continuous	Continuous or Tree Well	Continuous
<b>Min. Width (feet)</b>	n/a	4 – path 6 – sidewalk	5	72 sq. ft. min. – tree well; 5 min.	6 – path 5 – sidewalk

**6.2.6 Standard Specification for Thoroughfare Construction**

All thoroughfares constructed within Doña Ana County shall comply with New Mexico Department of Transportation Standard Specification for Highway and Bridge Construction, as adopted and amended.

- a. All testing and construction inspections for all developments shall adhere to the guidelines as set forth in the New Mexico Department of Transportation Standard Specification for Highway and Bridge Construction, as adopted and amended.
- b. A pavement design report included with the final plat or final document submittal shall include, but not be limited to:
  - i. Soil profiles shall be obtained from soil borings performed within the proposed right-of-way and proposed borrow area, or in areas determined to yield representative soils. Thoroughfare soil borings shall be taken at an interval of 500 feet. Additional soil borings may be required if soil characteristics change significantly. Soil borings shall be advanced to a depth of at least 5 feet, or refusal, whichever is shallower. In "cut" portions of the thoroughfare, the depth of borings shall be measured from the proposed finished grade elevation of the pavement. In "fill" portions of the thoroughfare, the depth of borings shall be measured below existing grade. Soil borings are required to classify materials to determine the thickness of the layers of the road structure and to identify soils that are excessively expansive or resilient. Such soils shall receive special consideration by substitution with "engineered fill" or stabilization with a suitable admixture and/or mechanical means. Field conditions encountered with unsatisfactory materials shall be submitted with pavement design calculations along with