

Development Costs

In order to improve the New Mexico airport system's performance, the NMASPU identified projects needed at all 51 study airports. Project development costs are those that may be incurred to improve the performance of the system, to meet identified targets, to correct deficiencies noted for facility and service objectives, and to implement current airport capital improvement plans (CIPs). Projects identified through the NMASPU process are quantified separately from the CIP costs, with short (5-year) and long (6-

20 year) term estimates provided for each airport. The cost estimates are to a planning level of detail and summarize the general financial requirements for the entire airport system. The costs developed are in 2009 dollars and do not account for inflation. Doña Ana County's complete list of NMASPU and CIP projects are listed in the NMASPU document.

The table below summarizes the estimated costs necessary to complete the projects identified for Doña Ana County At Santa Teresa.

Short Term		
NMASPU	CIP	Total
\$8,900,000	\$13,866,000	\$22,766,000
Long Term		
NMASPU	CIP	Total
\$4,320,000	\$0	\$4,320,000

TOTALS		
NMASPU	CIP	Total
\$13,220,000	\$13,866,000	\$27,086,000

The cost estimates are to a planning level of detail and summarize the general financial requirements for airport development.

NEW MEXICO DEPARTMENT OF TRANSPORTATION AVIATION DIVISION

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Doña Ana County Airport At Santa Teresa

NEW MEXICO AIRPORT SYSTEM PLAN UPDATE 2009



New Mexico DEPARTMENT OF TRANSPORTATION



Introduction

New Mexico's Airport System serves as an integral connection between New Mexico's businesses and residents, and the expanding global economy. The New Mexico Department of Transportation's Aviation Division has long recognized the importance of planning as a proactive approach to ensuring the airport system is being developed in a manner that best serves the people of New Mexico. In 2008, the Aviation Division initiated an update of its previous system plan for its system of 51 public-use airports. The New Mexico Airport System Plan Update (NMAASPU) provides a crucial document that summarizes the needs of New Mexico's airports and provides each airport with valuable information regarding the economic importance of the airport, land use protection, project and funding needs, and the value of the airport as an asset.

The update to the NMAASPU includes the following components:

- State Airport System Plan
- Economic Impact Analysis
- Land Use Drawing
- Asset Analysis

Summaries of relevant information from each of these components are provided in this document. A

full version of the NMAASPU can be obtained from the Aviation Division.

Airport Role

Doña Ana County At Santa Teresa has been identified as a Regional General Aviation airport. Regional General Aviation airports serve primarily general aviation activity, with a focus on serving business activity including jet and multi-engine aircraft. These airports support the system of Commercial Service airports and should provide significant coverage to the state's population. The FAA's National Plan of Integrated Airport Systems (NPIAS) uses a General Aviation classification for airports similar to Doña Ana County At Santa Teresa.

Facility and Service Summary

For system planning purposes, six airport role categories were developed to determine the level of facilities and services needed for the entire airport system to function at its highest level. These objectives provide guidance on the minimum level of facilities and services needed for each airport to best fulfill its identified role within the system. Doña Ana County At Santa Teresa Airport has a single runway, Runway 10/28, that is 8,500 feet long.

The following summarizes current facilities and services, the airport's future system recommendations, and projects recommended to meet the objectives within the context of the NMAASPU. It is important to note that the system objectives are just that, objectives, and specific conditions at the airport may support other developments not noted in the NMAASPU. More detailed design, planning,

and environmental analysis for airports will be accomplished as part of individual master plans. All airport projects will be required to meet eligibility and justification guidelines before being eligible for funding.

Role Summary	NMAASPU Role	Regional GA
	Federal Role	General Aviation
	NPIAS	Yes
	Ownership	Public

NMDOT's Aviation Division has long recognized the importance of planning as a proactive approach to ensuring the airport system is being developed in a manner that best serves the people of New Mexico.

Asset Summary	
Total 2008 Value	Percentage Increase
\$58,651,000	27%

airport stakeholders, and public policy decision makers with an understanding of the value of their airport. The table above summarizes the total value of the airport and how much it increased from 2005.

Economic Impact

New Mexico's system of 51 public-use airports generates billions of dollars in economic activity and supports thousands of quality jobs. An economic impact study was prepared as part of the NMAASPU and shows that each airport in the State serves as an economic catalyst not only for their community but for the State as a whole. The total economic benefit of aviation activity in New Mexico was quantified in terms of employment, payroll, and output (economic activity). The analysis considers two rounds of impact: the first round of benefits from employees of on-airport businesses, indirect expenditures of visitors who arrive via the airports and the

construction related activity. The construction activity considers each airport's spending for capital projects, and was averaged over a three-year period. The second round of benefits is the additional economic impacts that occur as the first round of impacts "multiply" or are re-spent as they enter the State's economy. For example, when an airport employee uses their paycheck to purchase groceries, this money is re-spent as the grocer purchases the products to sell and employs people at the store. The first and second rounds of benefits are added to determine the total economic impact of each airport.

In addition to quantified economic benefits, Doña Ana County At Santa Teresa provides qualitative benefits to the local community. The Airport is the base of many corporate aircraft, which promote commerce by allowing companies in Santa Teresa and western El Paso to efficiently do business nationwide. Considerable economic

development is occurring at and near the Airport, including along the border with Mexico to the Airport's south. These developments are enhanced by the Airport's location and services provided by the eight on-site tenants. The Airport also has daily air cargo service and offers flight instruction to prospective students.

The table below depicts the economic impact for the airport and the state as a whole.

Doña Ana County At Santa Teresa Total* Aviation Impacts		
	Airport	Statewide
Employment	234	48,795
Payroll	\$7,511,400	\$1,286,420,600
Output	\$17,960,300	\$3,196,781,100

* The totals reflect both first and second round impacts.

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specific land use planning and encroachment issues as they seek to protect the airport from future incompatible development. The Airport Influence Areas used for New Mexico reflect the property most likely to have aircraft overflights, particularly on approach or departure. The suggested Airport Influence Area boundaries consider the following FAA clearance zones:

- Runway Protection Zone (RPZ)
- Part 77 Approach Surface
- Part 77 Horizontal Surface

The Airport Influence Area boundary is centered on the runway midpoint and its centerline but varies at each airport based on the instrument approach procedures to each runway end and the Airport Reference Code for each runway. The dimensions for these areas are discussed in an appendix to the NMAASPU document. Brief descriptions of the suggested levels of land use controls in each area are described below:

- **No Development Area:**
Development in this area should be strictly controlled and limited, to the greatest extent possible, to on-airport developments, and be closely coordinated. The height of any proposed development within this area should be reviewed through the Part 77 process to ensure existing and future Part 77 surfaces are not penetrated and to determine

if airport operations would be negatively impacted. Noise sensitive land uses such as residences, churches, schools, or hospitals, should not be developed within this area.

- **Limited Development Area:**
Developments that are noise sensitive or accommodate significant groups of people should be limited within this area. Avigation easements are encouraged within this area; at a minimum, notification should be required of property owners or potential buyers that the property falls within this Airport Influence Area. Tall structures should be submitted under the Part 77 airspace review process.
- **Controlled Development Area:**
All land use categories are allowable within this area with residential development having the lowest density possible. Notification of the property owner or potential buyers that the property falls within this Airport Influence Area should be required. Tall structures should be submitted under the Part 77 airspace review process.

Asset Analysis

A valuation of each New Mexico airport was developed based on current construction costs and existing assets. The 2008 valuation is an update of an analysis conducted in the previous system plan, adjusted for inflation and for infrastructure improvements between 2005 and 2008. Developing this estimate provides the New Mexico public,

In order to assist airports with understanding land use issues and the current circumstances of the individual airports, the Aviation Division desired a land use review for each of the system airports.

Doña Ana County At Santa Teresa

	Existing	Future System Objective *	Recommendation
AIRSIDE FACILITIES			
Primary Runway Length (Feet)	8500'	6490'	None
Primary Runway Width (Feet)	100'	100'	None
Primary Runway Surface	Asphalt	Paved	None
Taxiway Type	Full Parallel	Partial Parallel	None
Pavement Condition Index (PCI)	69	70 or greater	Runway Rehabilitation
Approach Type	Non-Precision	Non-Precision	None
Runway/Taxiway Lighting	MIRL, LITL	MIRL	None
Approach Lighting System	None	Not Applicable	None
VGSI	2-PAPI	VGSI	None
REILs	Yes	Not Applicable	None
Rotating Beacon (Visual Aids)	Yes	Rotating Beacon	None
Wind Cone (Visual Aids)	Yes	Lighted Wind Cone	None
Weather	None	AWOS or ASOS	Install AWOS or ASOS
RCO	None		
LANDSIDE FACILITIES			
Hangar Spaces	Yes	60% Based/25% Transient	None
Apron Spaces	Yes	40% Based/50% Transient	None
Terminal	Yes	Terminal	None
SERVICES			
FBO	Yes	Full-Service	None
Auto Parking	Yes	Auto Parking	None
Fuel	AvGas - Yes Jet A Fuel - Yes	AvGas and Jet A	None
Restrooms	Yes	Restrooms	None
Pilot's Lounge	Yes	Pilot's Lounge	None
Security Plan	None	Not Applicable	None
Ground Transportation	Yes	Courtesy Car	None

* The objective serves as a minimum based on each airport's role. If the airport's facilities exceed the minimum objective identified, there is no recommendation to change the facility.

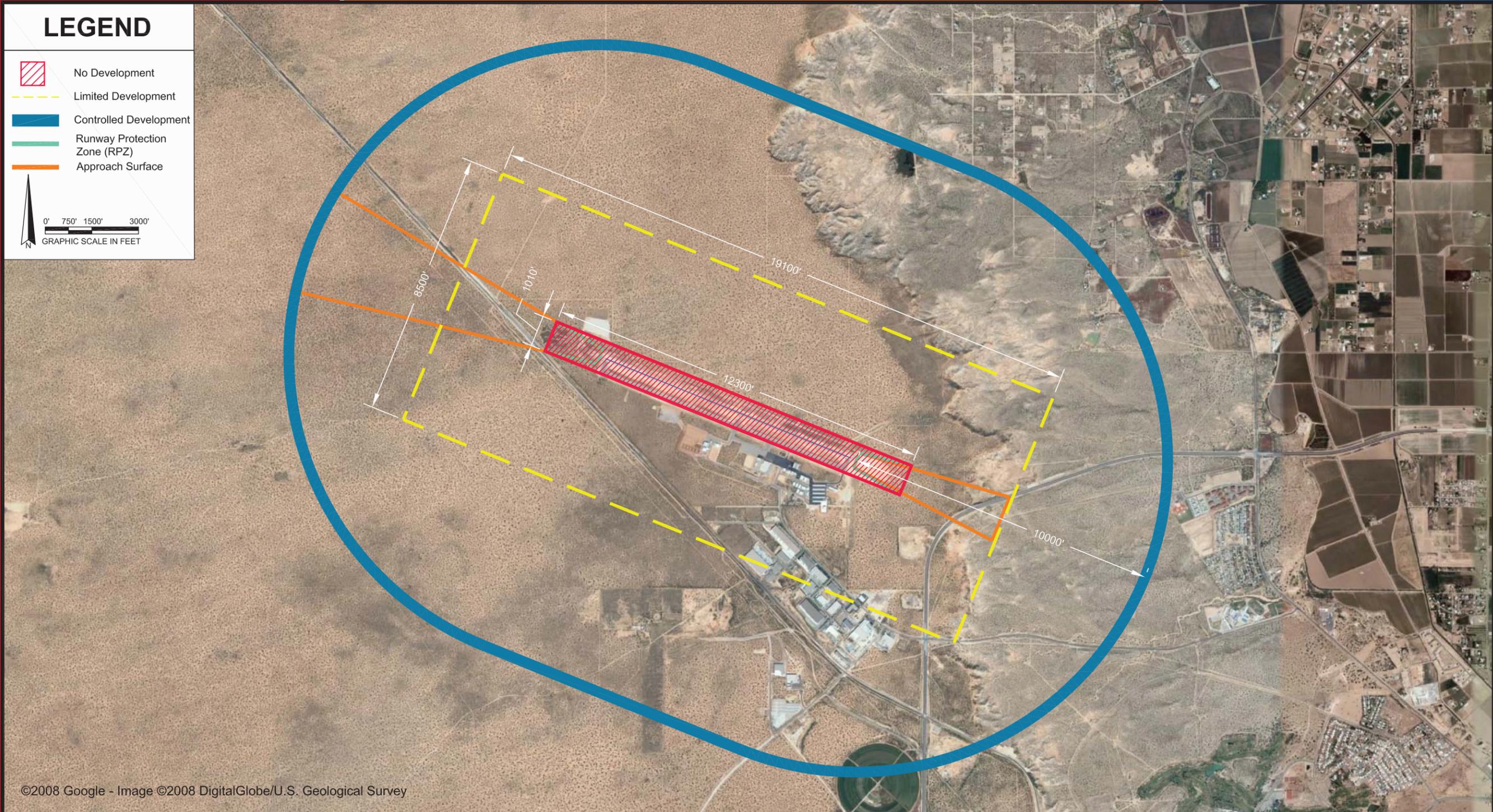
Land Use Evaluation

Land use compatibility guidance from the FAA is limited to the immediate vicinity of the runway and protection of airport airspace. This guidance is primarily safety related but does not address the potential for incompatible development near airports. In

order to assist airports with understanding land use issues and the current circumstances of the individual airports, the Aviation Division desired a land use review for each of the system airports. This review utilized generic "Airport Influence Areas" that represent a combination of similar land use planning techniques

utilized by other state aviation planning organizations.

A land use diagram was prepared for each study airport depicting a suggested "Airport Influence Area". These diagrams can be used by the State, airport management and local planning agencies to examine their



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2009 Airport System Plan



Doña Ana County Airport
Suggested Airport Influence Areas